

CITY OF WESTMINSTER			
PLANNING APPLICATIONS SUB COMMITTEE	Date 05.12.2023	Classification For General Release	
Report of Director of Town Planning & Building Control		Ward(s) involved Hyde Park	
Subject of Report	144 Praed Street, London, W2 1HU		
Proposal	Replacement of mansard roof with sheer brickwork extension, new mansard roof extension & roof top plant enclosure to provide additional Hotel Accommodation (Class C1). Alterations to façade fenestration and shopfronts to the building, installation of green wall to side (north east), and associated works.		
Agent	Iceni Projects		
On behalf of	LTH (Praed Street) Ltd		
Registered Number	22/08247/FULL	Date amended/ completed	17 August 2023
Date Application Received	5 December 2022		
Historic Building Grade	Unlisted		
Conservation Area	Bayswater		
Neighbourhood Plan	Not applicable		

1. RECOMMENDATION

Grant conditional permission, subject to a Grampian condition to secure mitigation for the demand for cycle parking.

2. SUMMARY & KEY CONSIDERATIONS

The application proposes extensions and alterations to this unlisted building of merit within the Bayswater Conservation Area, to provide additional hotel and restaurant floorspace within the Central Activities Zone (CAZ), Paddington Opportunity Area (POA) and Praed Street District Shopping Area, which is acceptable in principle in land use terms.

The key consideration in this case is:-

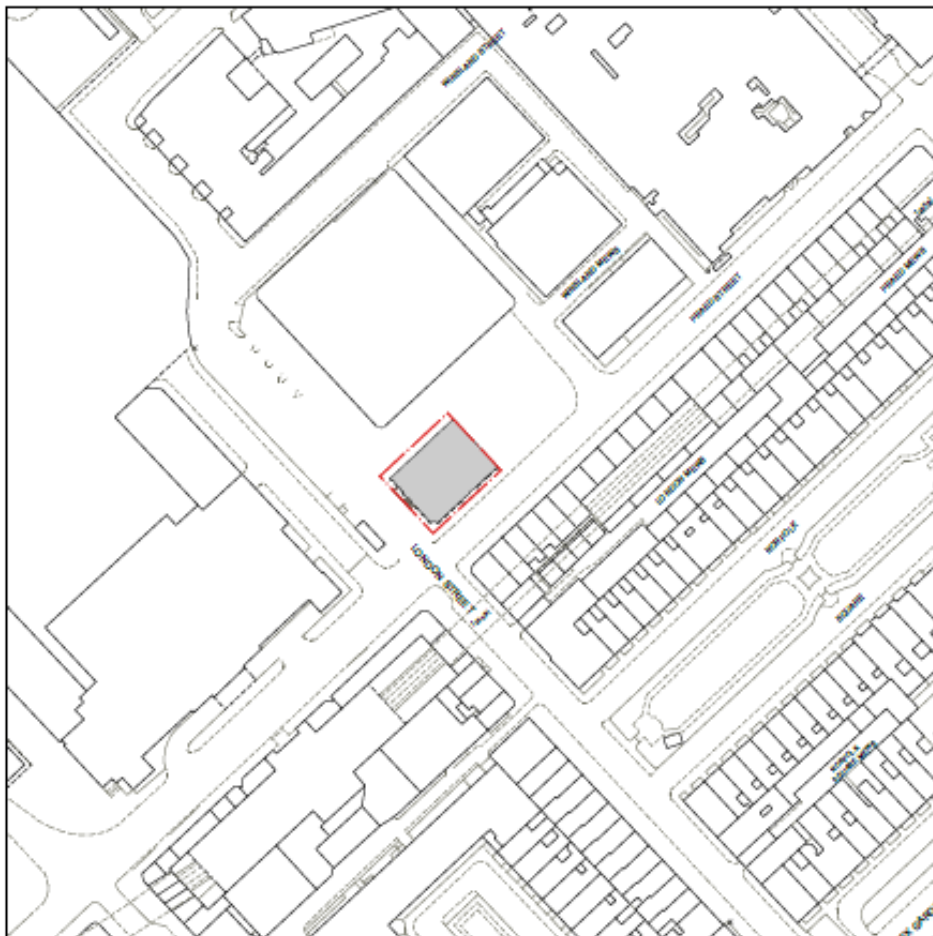
- The acceptability of the proposed extensions and alterations on the character and appearance of this unlisted building of merit and the Bayswater Conservation Area and the setting of other nearby designated heritage assets, such as the grade II listed buildings

adjoining the site.

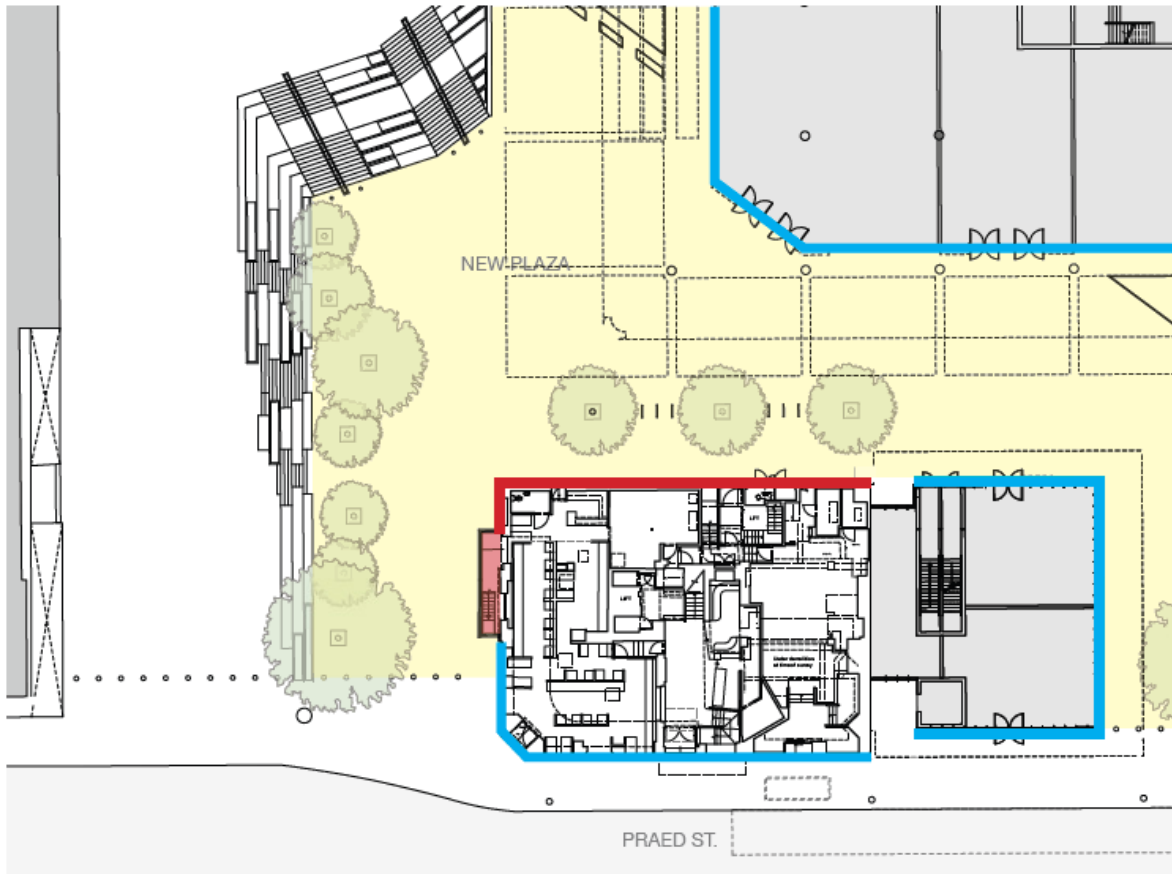
The proposal has been revised during the course of the application on the advice of officers, and in response to the concerns raised by Historic England and two local amenity groups, resulting in, among other things, a proposal focused on the existing traditional architectural character of the building and the upgrading to a sustainable building of BREEAM excellent.

Whilst the amenity groups remaining concerns are acknowledged, officers are supportive of the proposal. The proposal is considered acceptable and would be consistent with the relevant policies in the City Plan 2019-2040 and London Plan 2021. It is recommended that planning permission is granted, subject to the recommended conditions, which are necessary to make the development acceptable.

3. LOCATION PLAN



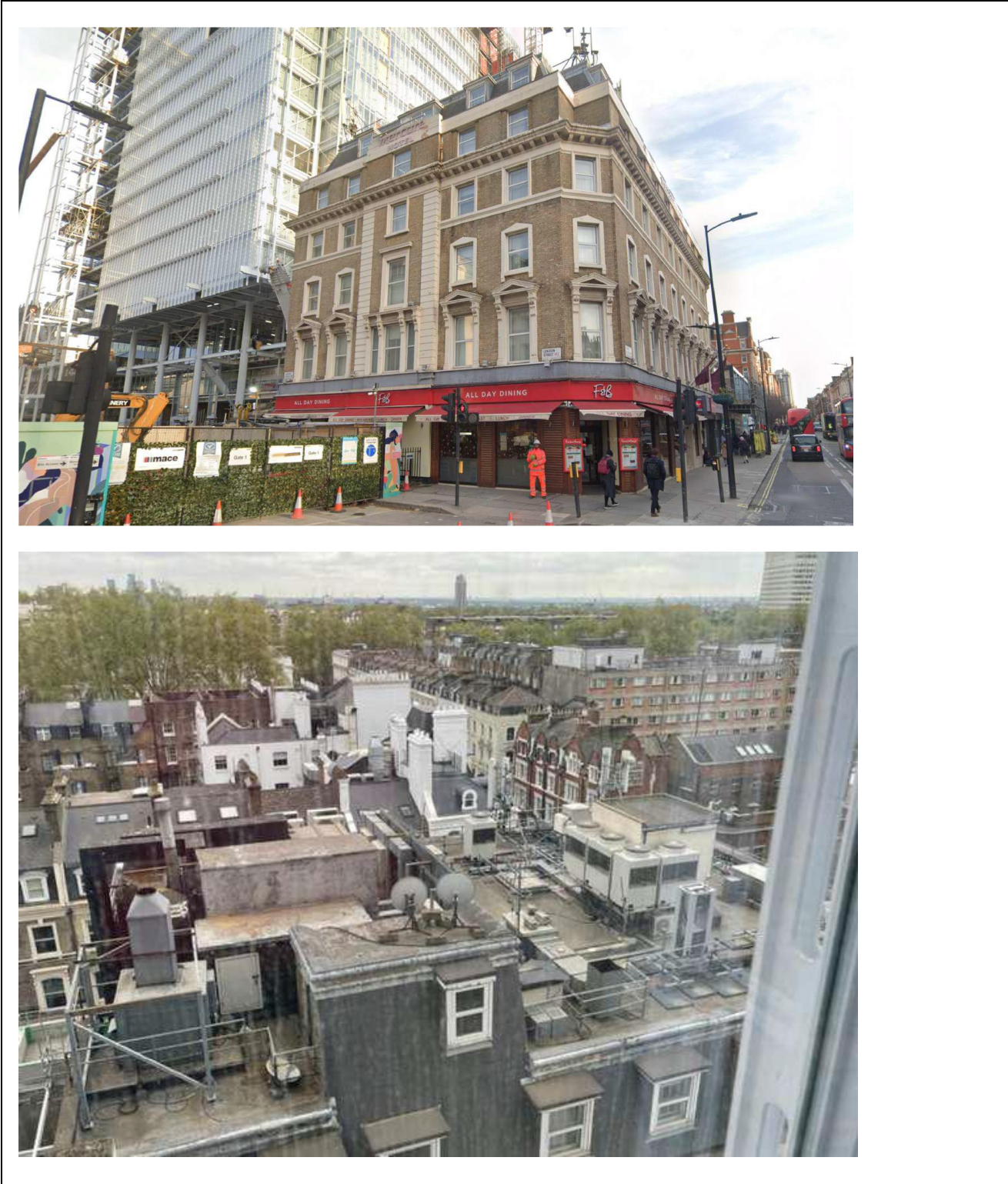
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Existing context plan

- | | |
|--|---|
| New Plaza | Fronts |
| Lightwell | Backs |

4. PHOTOGRAPHS



5. CONSULTATIONS

5.1 Application Consultations

ORIGINAL CONSULTATION FEBRUARY 2023

HISTORIC ENGLAND (LISTED BUILDS/CON AREAS)

Objection. The application does not meet the requirements of the NPPF, in particular paragraph numbers 199, 200 and 206. The proposals should be considerably revised, taking an architectural approach that reflects the sensitive heritage setting.

TRANSPORT FOR LONDON- Safeguarding LUL/DLR

No objection, subject to a condition to protect London Underground Transport Infrastructure.

TRANSPORT FOR LONDON- Spatial

Comment. Further information requested with respect to healthy streets and active travel, construction, delivery and servicing, trip generation, cycle parking, car parking, taxi provision.

NETWORK RAIL

Comment. Informatives with respect to safety and plant, scaffolding and cranes and interface with Network /Rail Assets.

FIRE HEALTH AND SAFETY EXECUTIVE (HSE)

No need to consult HSE, not a relevant building.

LONDON FIRE SERVICES

No response.

NHS NORTH WEST LONDON

Comment. As this does not relate to a residential development, NW London ICB will not be seeking a Section 106 contribution and we have no further comments to make.

HIGHWAYS PLANNING TEAM

No response.

WASTE PROJECT OFFICER

Objection. Further details required.

PLANT AND EQUIPMENT

No objection on environmental noise or nuisance grounds subject to conditions and informatives.

DESIGNING OUT CRIME

No response.

ARBORICULTURAL SECTION

Comment. Concerns and further information required. Relationship of green wall (steel support system and climbers on the north east elevation with the approved modular

green wall on the Praed Street building of the Cube, is unclear. Further information is requested to show these two elements in elevation and section. Single species for the green wall does not introduce visual interest. Landscape management plan is fairly general, query use of the basement lightwell.

ECONOMY TEAM

No response.

BUILDING CONTROL

No response.

WARD COUNCILLORS FOR HYDE PARK

No response.

PADDINGTON WATERWAYS & MAIDA VALE SOCIETY

No response.

PADDINGTON BUSINESS IMPROVEMENT DISTRICT (BID)

No response.

SOUTH EAST BAYSWATER RESIDENTS' ASSOCIATION

Objection. Design and conservation grounds (especially on the design of the elevations), increased height, failure to preserve the character of the Conservation area and damage to the setting of the adjacent listed buildings.

- Additional sheer facade and mansard should use traditional materials.
- Additional height alters the existing satisfying proportions of the building, especially as seen in long views, for instance from the corner of Spring Street and Praed Street.
- Proposed new elevations have a design and appearance that is detrimental to the characteristics of the vicinity.
- Request Section 106 agreement to prevent telecommunications equipment on the roof.
- Removal of all the existing decorative features on the two principal facades harmful and should be retained.
- Changes to building appearance fail to preserve or enhance the character and appearance of the building in relation to other buildings (including listed buildings) nearby.
- Bland appearance of no interest whatsoever and does not fit in with any part of its surrounds.
- Its appearance is worsened by new window glass going over the entire width and height of the opening, which is featureless.
- Window openings in the existing floors should remain as now.
- Green wall plants should be well maintained.
- Existing windows and surrounds should be retained & any new double glazing should be secondary or 'slimline' within the existing window frames.
- The windows in the new sheer floor should be similar to those in the storey immediately below, and should look the same-detail should be provided.
- French style windows are not good for sustainability, because of heating etc loss when open, losing the energy already used for heating and air conditioning of the

- room.
- Lack of window details and Juliette balconies for safety.
- Little increase in the number of rooms with facilities for disabled clients, or improvement in access to them.
- Insufficient information on shopfronts and their materials
- Request no advertising, especially not at high level on the principal facades.

HYDE PARK ESTATE ASSOCIATION

Objection.

Failure to preserve the character of the Conservation area and damage to the setting of the adjacent listed buildings. As far as HPEA is aware we were not consulted on this application due to its proximity we would expect to be so.

- Proposed changes fail to preserve or enhance the character and appearance of the building in relation to other buildings.
- Removal of the existing decorative features (window surrounds etc), to be replaced by bland facades, damages the setting of the nearby listed buildings.
- Existing windows should be retained, and secondary glazing added rather than double glazing.
- Any new floor to ceiling type modern windows if allowed on the ground floor should be fixed shut. Especially later during the evenings.
- Existing decorative features on the facades should be retained in keeping with the character of this building which adds to the present quirkiness of the old building.
- Query whether restaurant kitchen is large enough to store fridge freezers etc. will now be located ? as the kitchen does not look big enough.
- Presume the number of restaurant covers will be limited.
- Request that the stairs; leading to the hotel reception from Praed Street are appropriate and considered wide enough to deal with people carrying suitcases and increased amounts of visitors.
- Some merit in the roof line looking more neat and tidy. However, the sash windows are more appropriate and in keeping with the character and style of this building than the new suggested modern style. The design of the roof needs to continue to incorporate such quirkiness whilst at the same time as allowing a tidier roof line.
- Should accommodate a communal sitting area / bar area for at least 15 guests to have an alternative place to sit awaiting visitors, taxis, check ins etc. Instead of waiting outside this busy street with narrow pavements.
- Green walls are always welcome but this needs to incorporate a maintenance plan.

ADJOINING OWNERS/OCCUPIERS AND OTHER REPRESENTATIONS RECEIVED

No. Consulted: 54

Total No. of replies: 2

No. of objections: 0

No. in support: 1 and 1 requiring clarification

- This is a welcome change and will be significant in improving the landscape of Praed Street.

- Seek confirmation that the public realm surrounding the building is still to remain as per the approved plans under the Paddington Square permission, and in particular that no changes are proposed to be made to the existing arrangements of the north-western area (which directly faces the Cube building) at ground floor level and the interface with the public realm.

PRESS NOTICE/ SITE NOTICE:

Yes

RECONSULTATION AUGUST 2023 (A revised set of drawings and or further information with respect to the detailed design of the fenestration, interface with the new public realm of Paddington Square, sustainability and greening improvements, clarifications with respect to ground floor uses and existing rooftop telecommunication equipment, details of uplift in rooms, and DDA access and rooms and to address Transport for London queries).

HISTORIC ENGLAND (LISTED BUILDS/CON AREAS)

No objection. Historic England no longer objects to the application on heritage grounds. Other issues and safeguards outlined in our original advice and identified by your own specialist conservation advice may need to be addressed in order for the application to meet the requirements of the NPPF.

TRANSPORT FOR LONDON- Safeguarding LUL/DLR

No objection, subject to a condition to protect London Underground Transport Infrastructure.

TRANSPORT FOR LONDON- Spatial

Comment. TfL welcome the additional information and are now satisfied.

NETWORK RAIL

Comment. Informatives with respect to safety and plant, scaffolding and cranes and interface with Network /Rail Assets.

FIRE HEALTH AND SAFETY EXECUTIVE (HSE)

No need to consult HSE, not a relevant building.

LONDON FIRE SERVICES

Any response to be reported verbally.

NHS NORTH WEST LONDON

Comment. As this application relates to a hotel development, do not envisage any impact on health provisions

HIGHWAYS PLANNING TEAM

Informal advice, generally acceptable.

WASTE PROJECT OFFICER

No objection, subject to a permanency condition.

PLANT AND EQUIPMENT

No objection on environmental noise or nuisance grounds subject to conditions and informatives.

DESIGNING OUT CRIME

Any response to be reported verbally.

ARBORICULTURAL SECTION

Concerns remain that the existing modular green wall and the proposed steel support system and climbers would appear slightly incongruous together. It would be preferable to replace climbers with a modular green wall. If steel support and climbers is accepted, require further detail and information.

ECONOMY TEAM

Comment. Based on the total net uplift floorspace and on the Inclusive Local Economy Policy (2023), This scheme does not require any financial contribution or employment skills plan.

BUILDING CONTROL

Any response to be reported verbally.

WARD COUNCILLORS FOR HYDE PARK

Any response to be reported verbally.

PADDINGTON WATERWAYS & MAIDA VALE SOCIETY

Any response to be reported verbally.

PADDINGTON BUSINESS IMPROVEMENT DISTRICT (BID)

Any response to be reported verbally.

SOUTH EAST BAYSWATER RESIDENTS' ASSOCIATION

1st Response- Comment.

- Welcome the revised design of the facades to retain the existing design features around windows etc.
- Still have reservations on conservation grounds about the change in the original shape of the building, caused by the proposed addition of two floors.
- Extension will be seen in long views, for instance from the corner of Spring Street and Praed Street, so its design and appearance are important.
- Request Section 106 obligation to prevent equipment or additions to roof.
- The existing sash windows should keep their timber framed panes, restored as necessary & any double glazing should be secondary or 'slimline' within the existing window frames.
- Request a condition that the green wall plants be well maintained.
- Insufficient shopfront detail.
- Routes of construction vehicles, indicates one of two inbound routes being along the elevated A40, leaving it at the Paddington ramp. But this route is very restricted, not being available for vehicles over 7.5 tonnes.

2nd Response-Comment.

- The visuals now shown on the website strengthen our concern on design and

conservation grounds, especially in the prominent long views of the main facades of the building, such as that from the junction of Spring Street and Praed Street.

- The boundary around the telecom equipment (situated on top of the central flat section of the mansard roof) remain prominent in this street level view, despite some set back; thus accentuating the apparent increase in height of the building.
- Remain concerned also about the detail of the changes to the two principal facades:- on design grounds, for the new sheer facade and the dormer windows above it (including the materials to be used), and on conservation grounds, for the existing lower parts of these facades including the decorative window surrounds and the materials of the window frames. The latter should remain of timber and any double glazing of the panes should either be 'slimline' or secondary.
- Uncertainty about what is to be done to the existing ground floor shopfronts.

HYDE PARK ESTATE ASSOCIATION

Any response to be reported verbally.

ADJOINING OWNERS/OCCUPIERS AND OTHER REPRESENTATIONS RECEIVED

No. Consulted: 54

Total No. of replies: 0

No. of objections: 0

No. in support: 0

PRESS NOTICE/ SITE NOTICE:

Yes

5.2 Applicant's Pre-Application Community Engagement

The applicant has set out in section 3 of their planning statement, details of community engagement undertaken and provided an Early Community Engagement Plan. Collectively these documents indicate that they met with two representatives of Paddington Now Business Improvement District on 23.11.2022 whereby the planned extension and works to the Mercure Hotel were discussed with reference to design documents and presentation material. No other early community engagement (prior to the submission of the planning application) appears to have taken place.

The applicant's documents indicate that community engagement was to be undertaken during the course of the planning application and beyond, with the Southeast Bayswater Residents Association and Hyde Park Estate Association and Paddington Now Bid as the facilitator. A digital community engagement strategy is also to follow the application submission. The applicant advises that this will comprise a dedicated website that features the verified views of the building and an explanation of the proposed development. This website will be shared with the above-mentioned associations and BID and importantly, will also be made available to the wider community.

During the course of the application, the case officer made the applicant aware that that designated amenity society for the site is the Paddington, Waterways and Maida Vale Society and that the site was located within the Hyde Park Ward (for the purpose of

Ward Councillors). The applicant was also made aware of the omission of engagement with PWMVS, Ward Councillors, Paddington Square/Cube and St Mary's NHS/ Network Rail etc and residents and businesses. The applicant advised on 07.08.2023 that Icen Projects had been instructed to undertake engagement with the proposed stakeholders, to carry out a leaflet drop set up a consultation website and to facilitate further public community engagement and that a full Statement of Community Engagement (SCI) will be prepared to follow this.

The applicant provided an updated in an email of 5th October 2023. *“Stakeholder engagement undertaken during the determination period. The applicant held a community engagement event at the Mercure Hyde Park. The event was very well attended, with 60 local residents present in addition to local ward cllrs, local policy and members of SEBRA. The Applicant has reached out to SEBRA and Hyde Park residents association directly to brief them on the proposals. The response was very positive and local residents were excited to see proposals for an enhanced hotel offering and improved design along Praed Street”.*

No further information or SCI has been received. In summary, the applicant has not clearly set out details of early engagement with the local community and key stakeholders (apart from Paddington BID) in the area prior to the submission of the planning application, contrary to the principles set out in the Early Community Engagement guidance.

The scheme has however been revised during the course of the application.

6. WESTMINSTER'S DEVELOPMENT PLAN

6.1 City Plan 2019-2040 & London Plan

The City Plan 2019-2040 was adopted at Full Council on 21 April 2021. The policies in the City Plan 2019-2040 are consistent with national policy as set out in the National Planning Policy Framework (NPPF) (September 2023) and should be afforded full weight in accordance with paragraph 219 of the NPPF. Therefore, in accordance with Section 38 of the Planning and Compulsory Purchase Act 2004, it comprises the development plan for Westminster in combination with the London Plan, which was adopted by the Mayor of London in March 2021 and, where relevant, neighbourhood plans covering specific parts of the city (see further details in Section 6.2).

As set out in Section 38(6) of the Planning and Compulsory Purchase Act 2004 and paragraph 49 of the NPPF, the application must be determined in accordance with the development plan, unless material considerations indicate otherwise.

6.2 Neighbourhood Planning

The application site is not located within an area covered by a Neighbourhood Plan.

6.3 National Policy & Guidance

The City Plan 2019-2040 policies referred to in the consideration of this application have

been examined and have been found to be sound in accordance with tests set out in Paragraph 35 of the NPPF. They are considered to remain consistent with the policies in the NPPF (September 2023) unless stated otherwise.

7. BACKGROUND INFORMATION

7.1 The Application Site

No.144 Praed Street is a six storey plus basement building, occupied by the Mercure Hotel, and two ground floor restaurant operators (Garfunkel's restaurant and the now vacated Macdonald's). The site also houses roof level telecommunication apparatus. All four of the building's elevation are in public view, due to the adjacent Paddington Square development.

The site is located within the Central Activities Zone (CAZ), Paddington Opportunity Area (POA) and Praed Street is a District Shopping Centre. The site also falls within a Surface Water Flood Risk Hotspot (Westbourne). The site is well located for public transport for trains (Paddington Station) and London Underground (Bakerloo Line, Circle and District, Circle and Hammersmith and City and the Elizabeth Line) and buses with a Public Transport Accessibility Level (PTAL) of 6B.

The building on site is an unlisted building of merit, located within the Bayswater Conservation Area, sat amidst a townscape of high heritage value. To the north and east lies the site of the former Royal Mail Group post office which is currently under redevelopment for the 18-storey Paddington Cube/Square development (office, retail, restaurants) which includes the section of London Street between Praed Street and Winsland Street, Arrivals Ramp/Road to Paddington Station and the previously existing wall which separated these two roads.

Within the immediate vicinity of the site there are several listed buildings, notably the grade I Paddington Station, the grade II listed Hilton Paddington (formerly the Great Western Hotel), the grade II listed Paddington Underground Station and the grade II listed Mint and Clarence Wings of St Mary's Hospital.

The wider townscape includes a large number of designated heritage assets, including a big volume of grade II listed terraces within the largely nineteenth century townscape of the Bayswater Conservation Area. A number of other conservation areas are in close proximity.

7.2 Recent Relevant History

Telecommunication equipment

Appeal allowed on 14.05.2020 for temporary telecoms equipment at roof level comprising 3 x 3 metre high poles, 6 antennas, 1 x 0.3 metre dish, 1 x 0.2 metre dish, 4 cabinets and ancillary development thereto for a temporary period of 12 months. (19/06143/FULL). (The temporary telecommunications equipment was erected following the loss of permanent equipment at 50 Eastbourne Terrace due to redevelopment).

Conditional planning permission granted on 09.06.2021 for the siting of an existing temporary telecommunications base station for 12 months, comprising 6 antennas, 1

300mm diameter dish antenna, 1 200mm diameter dish antenna, mounted on 3 support poles, associated 4 rooftop mounted cabinets and ancillary works.(21/02422/FULL).

Conditional planning permission granted on 01.08.2022 Rooftop apparatus comprising; 3 support poles supporting 6 pole mounted antennas, 1x 300mm dish, 1 x200mm diameter dish, 4 cabinets and ancillary works thereto for a temporary period of 12 months. (22/03681/FULL).

Submitted (currently invalid) application for Installation of rooftop apparatus comprising; 3 no. antenna support poles supporting 6 no. pole mounted antennas, 1 no. 300mm dish, 1 no. 200mm diameter dish, 4 no. cabinets and ancillary works thereto. (23/04374/FULL).

8. THE PROPOSAL

Planning permission is sought for a scheme of retention of the building with some demolition, extension and alteration for continued hotel with ground floor restaurant use.

The proposal has been revised during the course of the application to address officer's concerns and queries as well as those raised by Historic England, two local amenity groups and transport for London. This resulted in a revised set of drawings and or further information with respect to:- the detailed design of the fenestration, interface with the new public realm of Paddington Square, sustainability and greening improvements, clarifications with respect to ground floor uses and existing rooftop telecommunication equipment, details of uplift in rooms, and DDA access and rooms. A further update to community engagement was also sought.

The key elements of the proposal are listed below:-

- Replacement of mansard roof with sheer brickwork extension
- New mansard roof extension
- Roof top plant enclosure
- Additional Hotel Accommodation (Class C1) and additional ground floor restaurant floorspace.
- Alterations to façade fenestration and shopfronts to the building
- Upgrading plant and services
- Installation of green wall to side (north east), and associated works.

Table: Existing and proposed land uses.

Land Use	Existing GIA (sqm)	Proposed GIA (sqm)	+/-
Hotel	2462m2	2804m2	+ 342
Class E	289m2	333m2	+44
Total	2751m2	3,137m2	+386 (12% increase)

9. DETAILED CONSIDERATIONS

9.1 Land Use

Mixed Hotel/Commercial use

The principle of a larger hotel (Class C1) with restaurant uses (Class E) in this location within the CAZ, POA and District Centre is acceptable in land use terms under policy 1 (Westminster's Spatial Strategy), policy 3 (Spatial Development Priorities: Paddington Opportunity Area, policy 14 (Town Centres, High streets and the CAZ), 15 (Visitor Economy), 16 (Food, Drink and Entertainment). This is in order to help create employment opportunities and diversify the local economy. This is, however, subject to the new hotel being of appropriate scale to the surroundings, having no significant adverse effects on residential amenity and generating no adverse environmental and traffic effects.

Hotel

The increase in floorspace provided for by the proposed upward extensions, is modest (342m²) and would provide an increase in 14 hotel rooms from 86 to 100. Some existing hotel rooms at 3rd, 4th and 5th floor levels are proposed to be reconfigured to enable the provision of 3 DDA wheelchair accessible hotel rooms (24m²). Increased accessibility through improvements to the main entrance are also proposed, including a new wheelchair platform lift and new internal ramps within the ground floor. These improvements are welcomed and the modest increase in hotel rooms to provide additional and improved visitor accommodation within this central location with excellent public transport accessibility level (Ptal 6) is supported by policy.

Class E restaurant

A larger restaurant is proposed to be retained on the ground floor, growing from 289m² to 333m². This modest increase is considered acceptable in this location and will not result in any significant impact to that existing. Given the existing restaurant use is retained, the small increase in floorspace proposed, the imposition of restrictive conditions (number of covers, hours of use etc) is not considered to be justified in this instance. The existing restaurant is subject to licensing and the slightly larger restaurant will also be subject to licensing controls. The request by the HPEA, is therefore not supported in this instance.

9.2 Environment & Sustainability

Whilst the proposal would be classed as a non-major development, it would still be expected to be of sustainable design. The proposed strategy is based on the principles of energy hierarchy- to use less energy, supply energy efficiently and use renewable energy (Be Lean, Be Clean, Be Green).

The proposal involves the substantial retention of the existing building (with some demolition) together with upward extensions and alterations to the building, which is welcomed and will help conserve resources, reduced embodied carbon, minimise waste and avoid dust and emissions from significant demolition. Sustainable design principles and measures incorporated into the design, will achieve at least BREEAM "Excellent" or equivalent standard. These features include:-

- Fabric first approach with insulation and air tightness to minimise heat loss.
- Energy efficient systems and equipment. Mechanical ventilation with heat recovery to the new extension.
- Metered energy use.
- Low energy LED lighting.
- Air Source Heat Pumps (ASHPs) for comfort cooling and heating and for part generation of hot water for the hotel. A single unit is to serve the ground and basement and one heat pump per hotel floor to the upper floors, together with a heat pump for the hotel handling unit.
- Water efficient fittings to minimise water consumption (low water use fittings, metering system, leak detection system)
- Material selection in consideration of BRE's Green guide to specification (low embodied impact, non-toxic, robust materials) and use of sustainably certified timber.
- Construction waste minimised and 80-90% diverted from landfill.
- Site waste management plan to be implemented.

As such the proposed development would reduce on-site energy demand and maximise the use of low carbon energy sources whilst extending the lifetime of the building by incorporating principles of sustainable design, in accordance with Policy 36 (Energy) and Policy 38 (Design principle).

Air Quality

The whole of Westminster is an Air Quality Management Area (AQMA). Given the scale and nature of the proposal, an air quality assessment was not required to be submitted. The site has a Public Transport Accessibility Level (Ptal 6), and no car parking is proposed on the site. The delivery and servicing strategy is proposed to be consolidated to seek to reduce daily servicing trips and the proposal incorporates improved green energy (ASHP's). As such, the proposal is not considered to result in any significant impact on air quality in accordance with Policy 32. Air quality.

Flood Risk & Sustainable Drainage

The site falls within flood Zone 1 and the site has a very low risk of flooding from tidal, fluvial flooding. Whilst located within a Surface Water Flood risk hotspot, given the retention of the building and nature of the proposal it is not considered to raise concerns with respect to pluvial (surface water) flooding. The proposal is therefore considered to comply Policy 35. Flood risk.

Environment & Sustainability Summary

Overall, the proposed environmental and sustainability credentials of the proposal are considered to be acceptable, taking into consideration the scale and nature of the proposal. Conditions are recommended to secure delivery of BREEAM excellent.

9.3 Biodiversity & Greening

The footprint of the existing building, which is to be retained, extended and altered, covers the full site and does not currently contain any greening or biodiversity features. Whilst there are limited opportunities for greening to this existing unlisted building of merit with rooftop plant, the proposal does seek to incorporate a green wall to the east elevation comprising of:-

- Steel support frame 4.8m high
- Plants: 2 species arranged as a block of 5 Clematis armandii (evergreen), and a block of 4 Clematis montana (deciduous) at about 500mm spacing.
- Initial planting height is 1-1.5m.
- Some additional species of ground cover plants to shade the roots of the climbers have also been provided.

Policy 34. Green infrastructure requires developments, wherever possible to contribute to the greening of Westminster and achieve biodiversity net gain wherever feasible and appropriate. The proposed green wall satisfies this policy and is welcome in principle.

During the course of the application, the applicant was requested to consider a modular green wall, rather than the proposed steel support frame for climbing plants, in order to provide for a better relationship with the adjacent modular green wall to be installed as part of the Paddington Cube Development (the smaller building on Praed Street).

However, instead they provided contextual information with respect to that relationship and have revised the green wall details to a taller structural frame (4.8m) and more details of plant species. The City Council's Arboricultural Manager remains concerned with respect to the steel support frame and climbers, with a preference for a modular green wall. As such, officers have again raised this issue with the applicant and any response will be reported verbally. However, given the scale and nature of the development, it is considered difficult to withhold permission on this specific ground.

The specifics of the currently proposed green wall (structural frame with climbing plants) require further refinement and as such a condition is recommended to seek the following revisions:-

- Planting arrangement - intimate mix preferable
- Planting density of climbers should be increased.
- Initial planting height of climbers should be increased.
- Species - suggest 3 species of climbers would be appropriate, 2 of which should be evergreen.
- Grid arrangement - diagonal grid of wires not visually appropriate.
- Detailed Management Plan required including access arrangements and sustainable irrigation.

Subject to this condition, whilst not the preferred green wall system, it will add greening and biodiversity to the site in accordance with policy.

The public realm, landscaping and trees associated with the Paddington Square Development are not proposed to be altered by this proposal.

9.4 Townscape, Design & Heritage Impact

Legislative & Policy Context

The key legislative requirements in respect to designated heritage assets are as follows:

Section 16 of the Planning (Listed Buildings and Conservation Areas) Act 1990 ('the

LBCA Act') requires that *"In considering whether to grant listed building consent for any works the local planning authority or the Secretary of State shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses."*

Section 66 of the LBCA Act requires that *"In considering whether to grant planning permission for development which affects a listed building or its setting, the local planning authority or, as the case may be, the Secretary of State shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses."*

Section 72 of the LBCA Act requires that *"In the exercise, with respect to any buildings or other land in a conservation area...special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area."*

Furthermore Chapters 12 and 16 of the NPPF require great weight be placed on design quality and the preservation of designated heritage assets including their setting. Chapter 16 of the NPPF clarifies that harmful proposals should be clearly and convincingly justified and should only be approved where the harm caused would be clearly outweighed by the public benefits of the scheme, including where appropriate securing the optimum viable use of the heritage asset, taking into account the statutory duty to have special regard or pay special attention, as relevant. This should also take into account the relative significance of the affected asset and the severity of the harm caused.

Being within a conservation area, Section 72 of the Act requires that decisions made for development proposals on the site must pay special attention to the preservation or enhancement of the character or appearance of the conservation area. Similarly, Section 66 of the same Act requires that the impact of development proposals on the setting of the nearby listed buildings must be given special regard or, in the terms of Section 16 of the NPPF, great weight when considering the overall effects of a scheme. In both respects, the relative significance of the affected heritage assets, including that of the application site itself and the manner in which it interacts with those other assets, should be taken into account.

Policy 38 (design principles), 39 (heritage) and 40 (townscape and architecture) provide the main framework for the application of the above-mentioned statutory and national requirements, whilst the Bayswater Conservation Area Audit SPD and Development and Demolition in Conservation Areas SPG provide valuable guidance to shape the quality and impact of new development in the area.

The Site and its significance

This unlisted building of merit within the Bayswater Conservation Area is of late 19th century date, but likely with origins as a hotel dating back to the arrival of the railway at least. It sits on a key junction between London Street and Praed Street, immediately adjacent to the main southern entrance to / exit from Paddington Station. It is the first building (other than those which form the main station complex itself), that one sees as you walk out of the Arrivals Road exit of the station, and as such forms an important 'first impression' moment for passengers upon their arrival into London. Whilst it is a building of historic character, undoubtedly deserving of preservation and respect, it has evidently

experienced a range of past alterations and extensions which have created the somewhat tired and unloved composition that we see today. Its original form and scale, commensurate with its separately built architectural cousin to the southern side of the junction (27 London St) remain evident, but upon closer inspection it becomes evident that its current height, and some of its external features are the product of later and in places, significant interventions.

The applicant's Heritage Statement provides a sound analysis of the age and development of the building's facades, showing that the 'original' or otherwise historic later elements of the building are quite degraded. Windows appear to be entirely later, whilst the upper sheer storey and roof are also later and of a lower standard of design merit than what we see on the southern side of the road. Shopfronts are entirely modern, and not particularly successful, whilst to the roof, the building has accumulated an unkempt array of modern telecoms and other plant. The building's 'rear' facades to the north and east were not originally intended for exposure and so now, post completion of the Paddington Square development, have had their more functional and less tidy faces revealed in a somewhat, unkind, isolating and exposing manner.

The site makes a strongly positive contribution to the character and appearance of the Bayswater Conservation Area. Directly opposite the site is the Grade II listed Hilton Hotel, which forms part of the wider station complex which dominates and substantially characterises this part of Praed Street. Paddington Station itself, including the former GWR office block fronting Arrivals Road, is a Grade I listed building of national significance. Also listed and nearby are the Grade II listed Paddington Underground Station, St Mary's Hospital's Mint Wing on London Street, and Clarence Wing further east up Praed Street. To the south of Praed Street, the character of the area changes quickly from the bustling station and hospital campus north of the road to a more domestic and quieter townscape of parallel streets of terraced housing characteristic of the Bayswater Conservation Area.

Immediately to the north and east of the site is the now effectively complete Paddington Square redevelopment site, consisting of a large multi-storey steel and glass 'Cube' office-led building set within an extensive area of new multi-level public realm and retail space, a new entrance to the Bakerloo Line LUL Station. This represents a substantial change to the character of the area which, whilst not changing the constraints of the site as such, do represent a step-change to its townscape context. The development has also fundamentally exposed the application site to a substantially more exposed position, with its northern and eastern facades now more public than before.

Bulk, Height & Scale & Detailed Design

It is proposed to substantially redevelop the building through its upward extension, and alterations to the exterior. This seeks to retain the bulk of the building's existing fabric, adding on top, two additional floors – one sheer, and one mansard. The proposals include no extensions at lower levels, the building retaining its existing footprint, but internally the building would be substantially remodelled to provide an enlarged and upgraded hotel, and improved retail spaces at ground floor level, including new shopfronts.

Revised during the course of the application on the advice of officers, and responding to the concerns raised by Historic England and two local amenity groups, the proposals are

now focussed on the existing traditional architectural character of the building, and depart from the somewhat sterile modernisation of the building's facades that had been previously proposed. This includes the retention of existing window openings to retained upper floors and selectively replacing and upgrading their decorative window surrounds, the retention of the prominent rusticated pilasters to the western façade extended upwards to terminate properly with a new pediment, and a new traditional double-pitched mansard roof on top. The proposals include some works also to the north and eastern facades, seeking to provide them with a more presentable face to these now more public approaches to the site. At ground level, the façade would be substantially reworked to create a new hotel entrance and wholesale replacement of shopfronts.

The main proposal is for the building's upwards extension by two storeys. This would involve the removal of the existing mansard roof, which is modern and of no significance, including the array of telecoms and other plant which currently dominate the building's roofline. This would be replaced by a new sheer storey matching the refurbished elevations below, and a new taller double-pitched 70:30 mansard. This new mansard would include a new rooftop plant and telecoms enclosure, concealed principally by the upper slope of the new mansard roof. To the east elevation, the inset lightwell of the existing building would be retained and again would contain extensive building plant but which would be concealed by a new high-level green wall. This proposal is consistent with measures required by condition of the adjacent Paddington Square development scheme, which had required that the untidy appearance of this lightwell be concealed from view.

The main impact of the proposed development would be the building's increase in height. This would be, in effect by two storeys, with one sheer storey replacing the existing low mansard, and a new taller mansard further on top of that. This would cause the building to rise noticeably taller than 27 London Street with which it forms a sort of 'paired' gateway relationship at the junction of Praed Street and London Street. This impact would be most apparent when viewed from the west, from the northern footway of Praed Street beside the main entrance to the Hilton Hotel, and from further west on Praed Street looking in the same direction. Here, the gateway effect of the existing application site building and 27 London Street forms a transition point between the western and eastern ends of Praed Street, and acting also as a turn directional junction towards the southern, more domestic character of Paddington South.

The new taller height of the application site would disrupt this effect somewhat, rising above the architectural height of no.27. However, this would be significantly mitigated by and compensated for by the proposed architectural approach of the development, preserving and enhancing the building's existing architecture, which would reinforce the site's coexistent appearance opposite no.27.

Through this traditionalist approach to design, the proposals have successfully maintained the site's transitory role in anchoring the traditional townscape that historically characterised the area despite the onset of the modern office developments immediately to the north and onwards across the rest of the Opportunity Area. The proposals are successful in their interpretation of the building's existing architecture, whilst also positively addressing its current limitations including lesser quality later alterations to some windows, to the ground floor and to the roof. The proposals to the north and east elevations, facing Paddington Square, are successful in their efforts to

provide some better face to these otherwise somewhat forgotten facades. A new entrance to the western end of the north elevation would help provide legibility to the restaurant entrance to passengers exiting the train stations.

The proposals include the replacement of all windows, alongside many of the existing stucco window surrounds, and other stucco details such as the main cornice between third and fourth floors. The applicants' analysis of these are supported by officers, in that they are mostly not of historic significance and are in many cases in fact quite poor quality. Where they are of historic interest, including at first floor and to the centre window of the west elevation, they are proposed by the revised scheme to be retained and enhanced. The proposals include for enhanced replacements of these to be installed, so lifting the historic character of the building and addressing many of its current architectural failings. The proposal to properly terminate the west elevation's rusticated pilasters with a new pediment is considered to be particularly welcome, providing a new proper western face to the building.

The wholesale replacement of the windows with new sash windows is also accepted in this case, and is generally consistent with the council's approach for unlisted buildings in conservation areas. The precise details of these new windows, which can be expected to include double-glazing, are to be secured through condition, as are the other new and refurbished architectural details proposed.

Also welcome is the proposed double-pitched mansard. Whilst tall, it is considered to be proportionate to the extended building below, and provides a positive means of visually enclosing the extensive plant. The additional plant projections above the main roofline would be set in from the ridgeline of the new mansard, such that it should not be readily visible from most ground level views. Where it could be visible, this might cause some harm to the building's appearance which should be weighed against the scheme's public benefits, including architectural. Conditions to secure the precise layout and appearance of this plant is recommended.

Impact on Heritage Assets

It is considered that some harm to the character of the conservation area would be caused by the proposal to increase the height of the building to a degree which would slightly depart from the established scale of buildings each side of this part of Praed Street. This would be at the lowest end of less than substantial harm. No harm would be caused to the setting of nearby listed buildings. However, significantly it is considered that this harm would be notably outweighed by the scheme's architectural benefits, in the form of architectural enhancements to all facades, and a careful and sympathetic approach to preserve what remains of significance, whilst successfully 'lifting' those elements which are currently unsuccessful. As such, overall, it can be considered that the development would preserve the significance of the conservation area, in that it would be overall undiminished and in many respects in fact enhanced.

The application is therefore considered to be compliant with the City Plan and adopted SPDs and SPGs, and a recommendation to grant conditional permission would be compliant with Section 72 of the Act and Section 16 of the NPPF.

The concerns raised by SEBRA and HPEA with respect to design and heritage concerns are in part addressed by officers in the above assessment and proposed conditions to

secure design details. However, the in principle objection to the principle of the height of the extension are not supported by officers for the reasons set out above.

Fire Safety

Given the scale of the development (non-major) and the nature of the use (hotel), the building is not considered as a relevant building to require a Fire Statement-Gateway One or consultation with the Health and Safety Executive (HSE), as confirmed by the HSE. Neither is a London Plan Fire statement required for this non-major application. Nevertheless, the applicant has provided a Fire Safety design approach. This includes fire detection and alarm, firefighters' lifts, evacuation lifts, smoke control, emergency lighting, emergency signage, first-aid firefighting and emergency power supplies. Early consideration of fire safety is welcome, and the development will still require compliance with building regulations.

Signage

The proposal does not include any details of signage. This is considered under separate legislation (Advertisement Regulations) and under a different type of application (Advertisement Consent). As such, no details are submitted as part of this proposal, and it is not appropriate to impose conditions relating to this matter as they are subject to separate controls. The request by SEBRA to impose a condition to prevent signage on the building, is for this reason not supported.

9.5 Residential Amenity

The site is surrounded on most sides by other commercial uses (Hilton Hotel, Paddington Station, Paddington Square). The closest residential occupiers are located on the opposite side of the road on the southern side of Praed Street, above the ground floor shops.

Daylight & Sunlight, Sense of Enclosure & Privacy

A Daylight and sunlight assessment has been submitted which assessed 63 windows that face the proposed development within the Hilton hotel and 153,155,157 and 161 Praed Street. All windows meet with the British Research Establishment's (BRE) Site Layout Planning for Daylight and Sunlight and would not see any significant loss of daylight or sunlight.

Given the location and scale of the proposed extension and the relationship with surrounding residential properties, the proposal is not considered to result in any significant increase in sense of enclosure or overlooking.

For the above reasons the amenity residents would be protected in accordance with Policy 7(A). Managing development for Westminster's people, Policy 33. Local environmental impacts and Policy 38 Design principles

Noise & Vibration

Roof top plant is proposed including air source heat pumps and the proposed kitchen extract ducting will be run internally through a fire rated shaft, up to roof level where a kitchen extract fan is located.

A Noise Impact Assessment has been undertaken by Finch Consulting to assess the likely cumulative impact of the proposed mechanical plant on nearby noise sensitive occupiers (Praed Street). The City Council's Environmental Officer is satisfied that the plant should satisfy the requirements of our standard noise conditions to protect residential amenity and consequently does not raise objection to the proposal on environmental noise or nuisance grounds. Our standard noise and vibration conditions are recommended to ensure ongoing compliance. Subject to these conditions, the proposal is considered to meet Policy 33. Local environmental impact and Policy 16 Food, drink and entertainment.

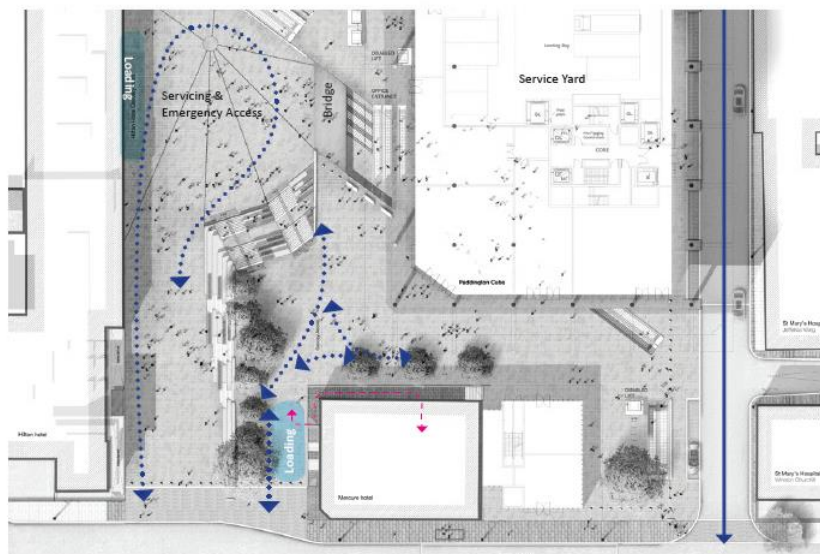
9.6 Transportation, Accessibility & Servicing

The site has a Public Transport Accessibility level (PTAL) rating of 6B with excellent access to many forms of public transport including National Rail, Elizabeth Line, London Underground and buses. In addition, Car club and cycle facilities are located within the vicinity of the site. A Taxi rank is located at Paddington Station.

A Transport Statement & Addendum and an Operational and Servicing Management Plan has been submitted in support of the proposal.

Servicing

Prior to the redevelopment of Paddington Square, the Mercure Hotel, was serviced on-street from Winsland Mews via London Street. In granting planning permission for the Paddington Square Development (Cube), this part of London Street was removed (in order to create the public realm area) and a new road (Tanner Lane) created further east within the site. It was agreed as part of Paddington Square development, that once completed, the Mercure Hotel and restaurant would be serviced from within the south east corner of the site (see below), which is retained as public highway for the purpose of servicing of the Mercure Hotel. No change is proposed to that arrangement under this current planning application to extend the Mercure Hotel.



Vehicular accessibility Strategy diagram from Design and Access Statement of The Cube by Renzo Piano Building Workshop

The applicant has provided an Operational Management Plan and Delivery and servicing information has been submitted within the Transport Assessment (TA) and addendum.

On the basis of deliveries / servicing being consolidated between the proposed restaurant and hotel. The proposed development would be expected to attract a reduced number of service vehicle trips, from the existing 8 daily visits to an average of 4 to 5 daily visits. This is acceptable under Policy 29. Freight and servicing and Policy 37 Waste management. Furthermore, TfL are now satisfied with the evidence provided that the applicant has access to the delivery space needed.

Highway Impact

A trip generation assessment was undertaken by RGP to understand the impact the development could have on the operation of the local highway network. The assessment found there to be a very minimal increase in the total number of trips, as well as the additional journeys made by public transport, and therefore the Transport Statement concluded that, overall, there would be a negligible impact on nearby pedestrian and public transport networks. The report concludes that it is not necessary to undertake any detailed junction capacity or public transport modelling. Officers agree with these conclusions and the proposal is not considered to have any significant highways impact above and beyond that of the existing use.

Accessibility

No car parking is proposed, and none is possible within the site in accordance with Policy 27 Parking. The applicant has indicated five blue badge areas within 150m of the site (London Street (south of Praed Street, Winsland Street and London Street north of Winsland Street).

Cycling & Cycle Storage

No cycle parking is proposed & the applicant has indicated that there is limited space within the existing ground floor layout to accommodate new cycle parking facilities. This is acknowledged, albeit regrettable. The Paddington Square development will include public cycle parking within the proposed public realm; however, this provision should not be relied upon for the non-provision under this application. The applicant has suggested that “ *it is anticipated that some additional cycle parking facilities could be secured within the public highway if necessary*”. As such a condition is recommended to secure mitigation for the lack of cycle parking provided as part of this development, so that this can be investigated and to ensure that the proposal seeks to comply with Policy 25. Walking and cycling and address the request of TFL.

Relationship with Paddington Square

No changes are proposed beyond the application site, to the adjacent public realm to the north of the building. The area immediately north outside of the application site is to be maintained as public highway, as stipulated within the S106 legal agreement to Paddington Cube. Therefore, the public realm, landscaping and trees associated with the Paddington Square Development are not proposed to be altered as part of this proposal.

LUL Infrastructure

There is London Underground tunnels and infrastructure in close proximity to the site. LUL have requested a condition to require by way of a condition, evidence to ensure that the proposal will not detrimentally impact on LUL infrastructure. Accordingly, a condition to this effect is recommended to ensure that the development does not impact on existing London Underground transport infrastructure, in accordance with London Plan 2021, draft London Plan policy T3 and ‘Land for Industry and Transport’ Supplementary Planning Guidance 2012

TFL

The site is located along Praed Street which is 175m from Sussex Gardens which forms part of the SRN (Strategic Road Network). While the City Council is also the Highway Authority for this road, TfL is the Traffic Authority and has a duty under the Traffic Management Act 2004 to ensure that any development does not have an adverse impact on the SRN.

Construction Logistics Plan

The applicant has submitted a draft Construction Logistics Plan (CLP) detailing measures that will be implemented to ensure that there is no adverse impact on the surrounding transport network and demonstrate how safe pedestrian and cyclist access around the boundary of the site should be maintained throughout the construction process and avoid adverse impacts to buses. A condition is recommended to seek a final CLP for approval in liaison with TFL.

Healthy Streets and Active Travel

TFL has requested that the applicant undertake an Active Travel Zone (ATZ) as a minimum a light touch review of the key pedestrian/cycle routes should be undertaken with a view to the developer funding / delivering enhancements to the public realm in the vicinity of the site, to support active travel and mode shift, in line with London Plan policies T1 and T2. The applicant has responded in their addendum Transport statement

with details of the ptal 6B rating, and setting out details of the public realm associated with the adjacent Paddington Square Development, which will provide for much improved environment for pedestrians and that hotel guests will be provided with a direct principal route from Paddington Station through the new public realm to the hotel. The applicant has also set out that an ATZ Assessment cannot be carried out at present due to the current works at Paddington Square and that the request is not proportionate to the scale of the proposal. Officers consider that the request for the ATZ is not commensurate with the scale of the proposal and also given that the proposed public realm to Paddington Square development is known to provide direct pedestrian links between major transport interchange at the hotel. As such, TFL's request is not supported.

9.7 Economy including Employment & Skills

The West End has been particularly hard hit by the pandemic and there is a need for businesses within the Central Activities Area to be supported at this time to enable their post pandemic recovery. The proposed development will contribute to the recovery of the West End in accordance with Policies 1 and 13 in the City Plan 2019-2040.

Whilst the development is of insufficient scale to require an employment and skills plan, it will contribute positively to the local economy. The new floorspace proposed is expected to generate additional jobs, the applicant expects the proposal to result in an uplift in the number of full time employees from 18 to 58.

- Front of house team (receptionists, porters,) **10**
- General Manager, Night Manager, Reservations Manager, Front of House Manager, HK Manager **5**
- Housekeeping staff in house **15**
- Maintenance team 3 operatives plus 1 facility manager **4**
- Introducing sales, marketing, e-commerce, conference & events team **4**
- Restaurant and Bar (kitchen and front of house) team – **20**
- Total - 58

The increase in jobs supported by this site will help to promote opportunities for local employment and will lead to increased spending in existing nearby shops and services and other town centre uses. The proposal will also be beneficial during the construction phase through the generation of increased opportunities for local employment, procurement and spending. This accords with Policy 1. Westminster's Spatial Strategy, Policy 13. Supporting Economic Growth and Policy 18. Education and skills.

Digital Infrastructure & Connectivity

It is acknowledged that existing telecommunication equipment is located on the roof of existing building. The applicant has indicated that it is not intended to re-install telecommunication equipment on the building post completion of the development.

It is acknowledged that digital infrastructure and connectivity support future economic growth as set out under Policy 19. Digital infrastructure, information and communications technology. However, given that the building is currently much more exposed than in the past due to the Paddington Square development it is considered appropriate to impose a condition to restrict telecommunication equipment being installed on the roof of this unlisted building of merit, without planning permission. This is because without full assessment as to the principle and siting and appearance of any such equipment, these

may harm the appearance of this unlisted building of merit, the setting of nearby listed buildings and the character and appearance of the Bayswater Conservation Area and area generally, including views from the new Paddington Square Public Realm, which are protected by Policies 38 and 40.

Whilst SEBRA have suggested the use of a S106 planning obligation rather than a condition, planning obligations should only be used where it is not possible to address unacceptable impacts through a planning condition.

9.8 Other Considerations

None.

9.9 Environmental Impact Assessment

The proposed development is not of sufficient scale or impact to require an Environmental Impact Assessment.

9.10 Planning Obligations & Pre-Commencement Conditions

Planning obligations are not relevant in the determination of this application.

The estimated Westminster CIL payment is **£76,043.36**, whilst the estimated Mayoral CIL payment is **£68,073.65**. Note that these figures exclude any discretionary relief or other exemptions that may apply and are estimates based on the floorspace identified in the submitted drawings and documents. The actual CIL liability will be calculated by our CIL & S106 Team post determination of the application using the process set out in the Community Infrastructure Levy Regulations 2010 (as amended).

The Town and Country Planning (Pre-commencement Conditions) Regulations 2018 requires the City Council to obtain the applicant's written agreement before imposing pre-commencement conditions (i.e., conditions which must be discharged before works can start on site) on a planning permission. Pre-commencement conditions can only be imposed without the written agreement of the applicant where the applicant fails to provide a substantive response within a 10 -day period following notification by the Council of the proposed condition, the reason and justification for the condition.

During the course of this application, a notice was served relating to the proposed imposition of a pre-commencement condition to secure the applicant's adherence to the following conditions:

Condition 3- LUL Infrastructure (LUL request)

Condition 4- Construction Logistics Plan (TFL request)

Condition13- Green Wall.

Condition 14- Mitigation for Cycle Parking.

The applicant's agreement to the imposition of the conditions are being sought prior to the committee meeting.

9.11 Assessment of Planning Balance

Assessment of the Planning Balance is not applicable, see section 9.4. Overall, the harm to heritage assets resultant from the height of the extension is outweighed by the architectural merits of the proposal and overall, the proposal is considered to preserve the significance of the Bayswater Conservation Area.

10. Conclusion

The proposal would increase visitor accommodation with improved access and accessible rooms, and additional restaurant space, all within a location with excellent public transport accessibility levels within the CAZ, POA and District Shopping centre, with minimal transport and highways impact. Furthermore, the proposed upgrading of the sustainability of the building to meet BREEAM Excellent and the introduction of greening is welcomed. Overall, in heritage terms, the proposal is considered to preserve the significance of the Bayswater Conservation Area in that it would be overall undiminished and in many respects in fact enhanced.

Accordingly, the proposal is considered acceptable and would be consistent with the relevant policies in the City Plan 2019-2040 and London Plan 2021. It is recommended that planning permission is granted, subject the conditions listed at the end of this report, which are necessary to make the development acceptable.

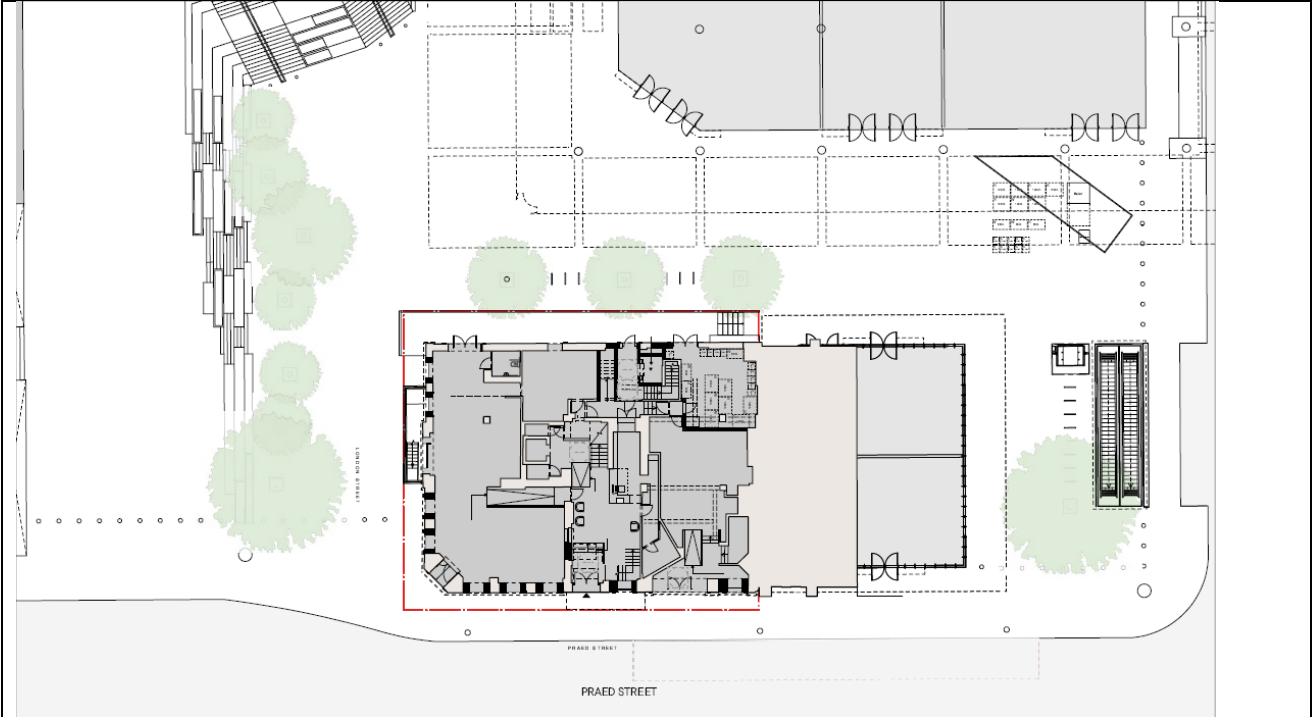
(Please note: All the application drawings and other relevant documents and Background Papers are available to view on the Council's website)

IF YOU HAVE ANY QUERIES ABOUT THIS REPORT, PLEASE CONTACT THE PRESENTING OFFICER: SARAH WHITNALL BY EMAIL AT swhitnall@westminstergov.uk.

11. KEY DRAWINGS



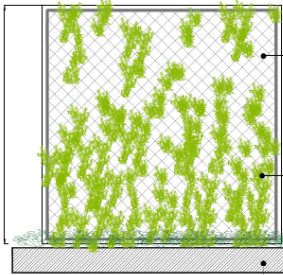
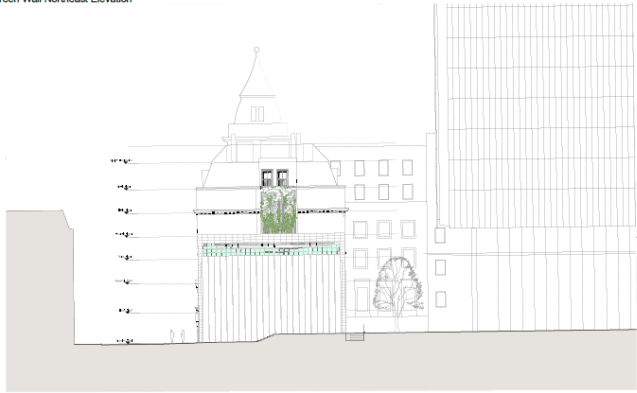




Inset Plan Green Wall Location



Green Wall Northeast Elevation



Jakobs Inox Line wire installed on steel anchoring system fixed directly to building facade to suppliers requirements.

Climbing plants trained to grow up line system offset 80mm from wall with a leaf-stem climber support, lattice size 150-250mm.

0.5 x 0.5m planter to be filled with 400mm premium grade top soil and organic compost with 50mm granular drainage layer to base.



Raised Aluminium trough planter with integrated irrigation system and drainage fixed to building facade. Street design Versatile Planters or similar. Powder coated colour finish to Client preference.

3 No. Bergenia 'Sunningdale'	3 No. Carex oshimensis 'Evergold'	3 No. Polystichum setiferum
2 No. Polystichum setiferum	3 No. Carex oshimensis 'Evergold'	3 No. Polystichum setiferum
3 No. Carex oshimensis 'Evergold'	3 No. Carex oshimensis 'Evergold'	3 No. Polystichum setiferum
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3 No. Carex oshimensis 'Evergold'	3 No. Carex oshimensis 'Evergold'	3 No. Polystichum setiferum

Planting Schedule	Plant	Quantity	Location	Notes
1	Bergenia 'Sunningdale'	3	Green Wall	Plant in 0.5m x 0.5m planters
1	Carex oshimensis 'Evergold'	3	Green Wall	Plant in 0.5m x 0.5m planters
1	Polystichum setiferum	3	Green Wall	Plant in 0.5m x 0.5m planters
1	Hardy pink clematis	4	Green Wall	Plant in 0.5m x 0.5m planters
1	Clematis montana nigra	4	Green Wall	Plant in 0.5m x 0.5m planters

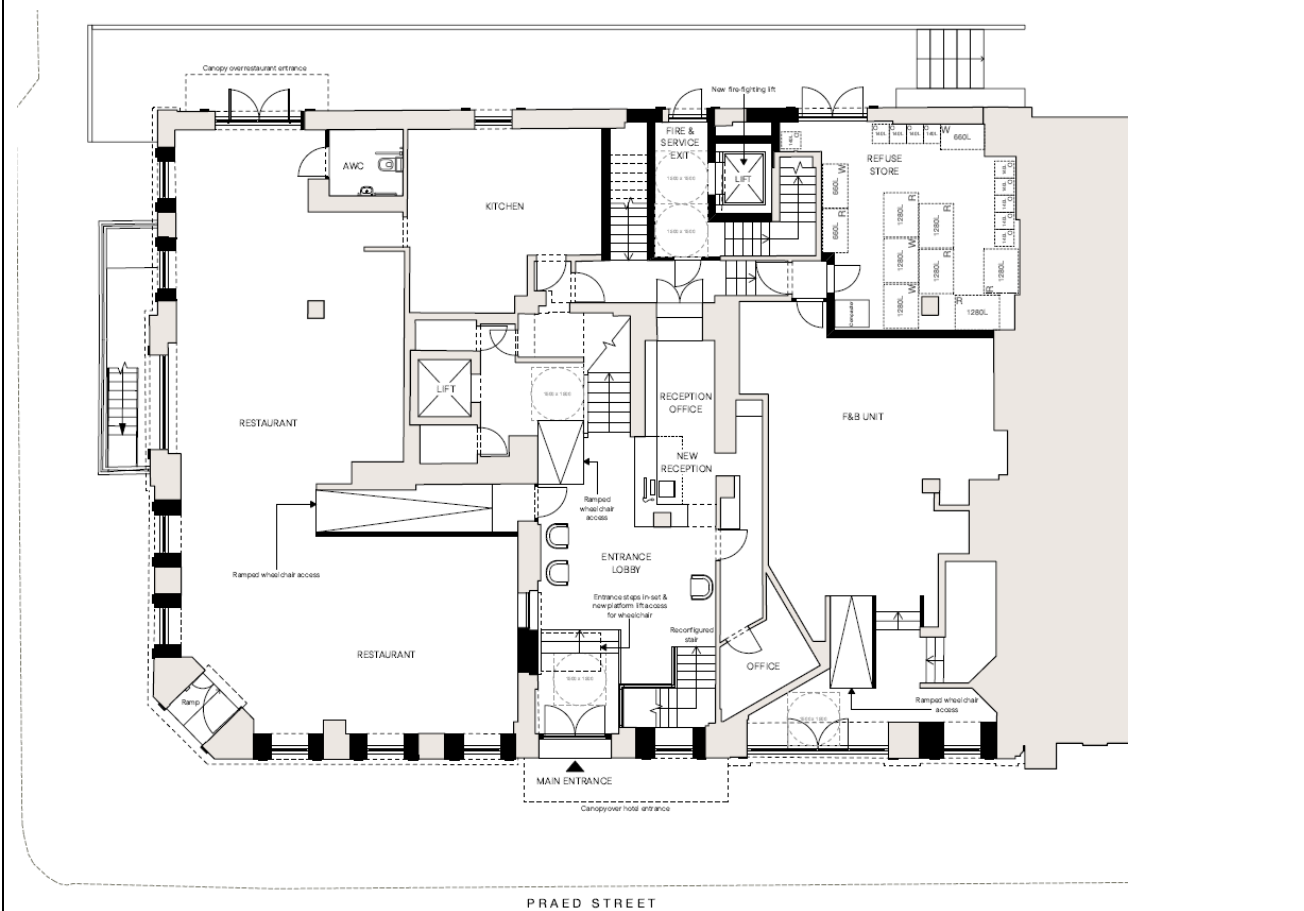
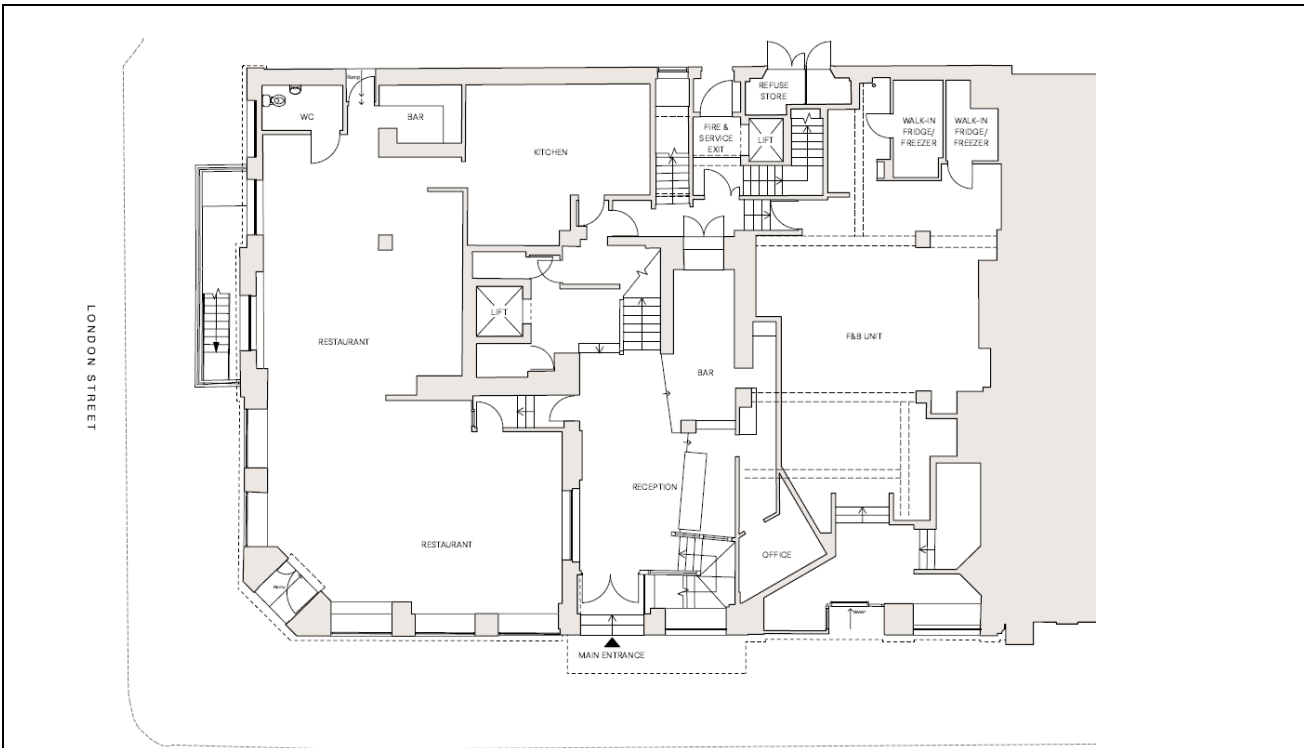
Site Plan
Green Wall
Green Wall
Green Wall

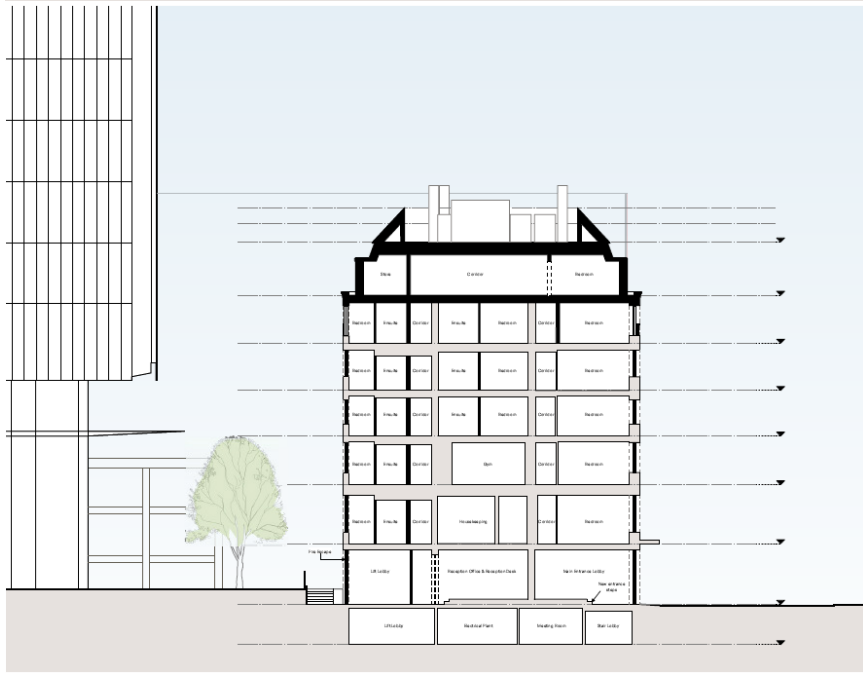
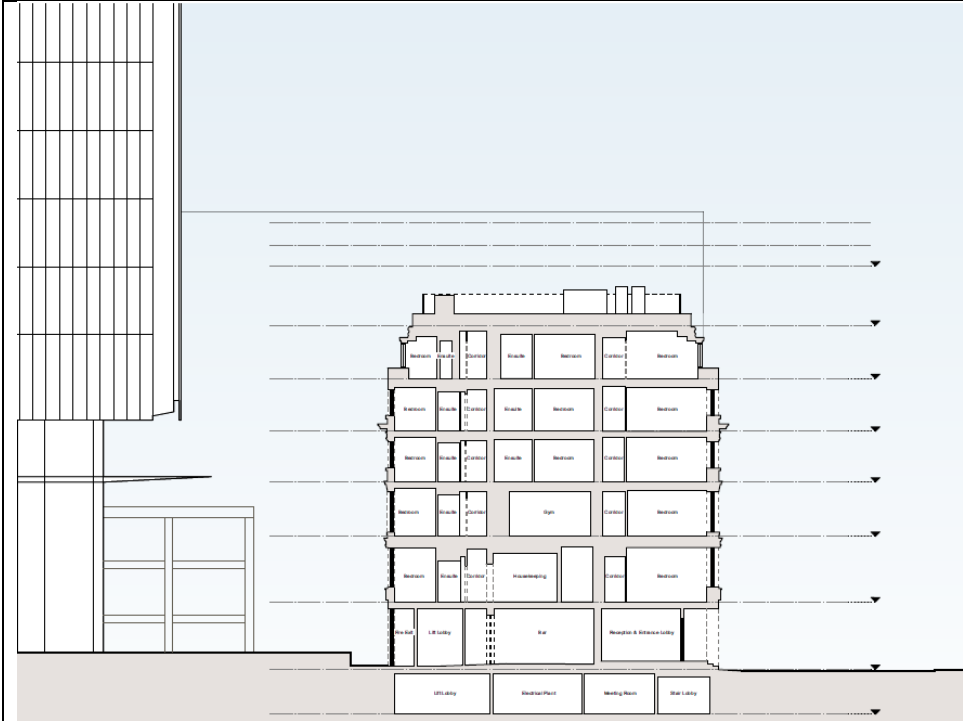
Aspect
Landscape
Landscape
Landscape

aspect *landscape planning*

Mercury Hotel, Paddington
Green Wall Planting Plan
Client: LTH (Praed Street) Ltd

02/2023 19/08/2023
WAG:ALJ
S164-PP 1.0





DRAFT DECISION LETTER

Address: 144 Praed Street, London, W2 1HU

Proposal: Replacement of mansard roof with sheer brickwork extension, new mansard roof extension & roof top plant enclosure to provide additional Hotel Accommodation (Class C1). Alterations to façade fenestration and shopfronts to the building, installation of green wall to side (north east), and associated works.

Reference: 22/08247/FULL

Plan Nos: EXISTING DRAWINGS, Location Plan PS_001, Site Plan PS_002, Basement Floor Plan PS_009, Ground Floor Plan PS_010A, First Floor Plan PS_011, Second Floor Plan PS_012, Third Floor Plan PS_013, Fourth Floor Plan PS_014, Fifth Floor Plan PS_015, Roof Plan PS_016, Southeast Elevation PS_020, Southwest Elevation PS_021, Existing Northwest Elevation PS_022, Existing Northeast Elevation PS_023, Existing Southeast Section PS_030, Existing Southwest Section PS_031

DEMOLITION, Basement Floor Plan PS_D-009, Ground Floor Plan PS_D-010 RevA, First Floor Plan PS_D-011, Second Floor Plan PS_D-012, Third Floor Plan PS_D-013, Fourth Floor Plan PS_D-014, Fifth Floor Plan PS_D-015, Roof Plan PS_D-016, Southeast Elevation PS_D-020 RevA, Southwest Elevation PS_D-021 Rev A, Northwest Elevation PS_D-022 RevA, Northeast Elevation PS_D-023

PROPOSED, Site Plan PS_050 Rev A, Basement Floor Plan PS_099, Ground Floor Plan PS_100 Rev B, First Floor Plan PS_101 Rev A, Second Floor Plan PS_102 Rev A, Third Floor Plan PS_103 Rev A, Fourth Floor Plan PS_104 Rev A, Fifth Floor Plan PS_105 Rev A, Sixth Floor Plan PS_106 Rev A, Plant Room Plan PS_107 Rev A, Roof Plan PS_108 Rev A, Southeast Elevation PS_200 Rev A, Proposed Southwest Elevation PS_201 Rev A, Proposed Northwest Elevation PS_202 Rev B, Proposed Northeast Elevation PS_203 Rev A, Proposed Southeast Section PS_300 Rev A, Proposed Southwest Section PS_301 Rev A

Planning Statement (230216 version) Iceni Projects, Covering Letter Iceni Projects, Design and Access Statement (230126 version) Matthew Lloyd Architects, Heritage and Townscape Visual Impact Assessment ('HTVIA') (230208 version) Iceni Projects, Transport Statement RGP and Addendum, Operational and Servicing Management Plan, Sustainable Design Statement Iceni Projects, Daylight and Sunlight Assessment Iceni Projects, BREEAM Pre-Assessment SCS Partnership Rev, Noise Impact Assessment Finch Consulting, Fire Statement Semper Group, Landscape Management Plan Aspect Landscape (for information only), Green Wall Planting Plan Aspect Landscape (for information only), Outline Construction Logistics plan (for information only)

Case Officer: Sarah Whitnall

Direct Tel. No. 020 7641
07866036375

Recommended Condition(s) and Reason(s)

- 1 The development hereby permitted shall be carried out in accordance with the drawings and other documents listed on this decision letter, and any drawings approved subsequently by the City Council as local planning authority pursuant to any conditions on this decision letter.

Reason:

For the avoidance of doubt and in the interests of proper planning.

- 2 Except for piling, excavation and demolition work, you must carry out any building work which can be heard at the boundary of the site only: , o between 08.00 and 18.00 Monday to Friday; , o between 08.00 and 13.00 on Saturday; and , o not at all on Sundays, bank holidays and public holidays. , , You must carry out piling, excavation and demolition work only: , o between 08.00 and 18.00 Monday to Friday; and , onot at all on Saturdays, Sundays, bank holidays and public holidays. , , Noisy work must not take place outside these hours unless otherwise agreed through a Control of Pollution Act 1974 section 61 prior consent in special circumstances (for example, to meet police traffic restrictions, in an emergency or in the interests of public safety). (C11AB)

Reason:

To protect the environment of neighbouring occupiers. This is as set out in Policies 7 and 33 of the City Plan 2019 - 2040 (April 2021). (R11AD)

- 3 The development hereby permitted shall not be commenced until the following in consultation with London Underground have been submitted to and approved in writing by the local planning authority which:
 1. provide demolition and construction details including all of the foundations, basement and ground floor structures, or for any other structures below ground level, including piling (temporary and permanent).
 2. accommodate the location of the existing London Underground structures and tunnels.
 3. accommodate ground movement arising from the development construction.
 4. mitigate the effects of noise and vibration arising from the adjoining railway operations within the structures and tunnels.

Reason:

To ensure that the development does not impact on existing London Underground transport infrastructure, in accordance with London Plan 2021, draft London Plan policy T3 and 'Land for Industry and Transport' Supplementary Planning Guidance 2012

- 4 No development shall take place, including any works of demolition, until a Construction Logistics Plan has been submitted to and approved in writing by the City Council as local planning authority in liaison with Transport for London. ,

Reason:

In order to appropriately manage any potential adverse effects on the Transport for London Road Network (TLRN) as requested by Transport for London.

- 5 Servicing of the site must be carried out in accordance with the servicing strategy set out within the Transport Statement its Addendum and Operational and Servicing Management Plan.

Reason:

To avoid blocking the surrounding streets and to protect the environment of people in neighbouring properties as set out in Policy 29 of the City Plan 2019 - 2040 (April 2021). (R23AD)

- 6 Before the use is commenced, you must provide the separate stores for waste and materials for recycling shown on drawing number 100 Rev B; prior to occupation and thereafter you must permanently retain them for the storage of waste and recycling. You must clearly mark them and make them available at all times to everyone using the building. (C14FC)

Reason:

To protect the environment and provide suitable storage for waste and materials for recycling as set out in Policies 7 and 37 of the City Plan 2019 - 2040 (April 2021). (R14CD)

- 7 (1) Where noise emitted from the proposed plant and machinery will not contain tones or will not be intermittent, the 'A' weighted sound pressure level from the plant and machinery (including non-emergency auxiliary plant and generators) hereby permitted, when operating at its noisiest, shall not at any time exceed a value of 10 dB below the minimum external background noise, at a point 1 metre outside any window of any residential and other noise sensitive property, unless and until a fixed maximum noise level is approved in writing by the City Council. The background level should be expressed in terms of the lowest LA90, 15 mins during the proposed hours of operation. The plant-specific noise level should be expressed as LAeqTm, and shall be representative of the plant operating at its maximum. , (2) Where noise emitted from the proposed plant and machinery will contain tones or will be intermittent, the 'A' weighted sound pressure level from the plant and machinery (including non-emergency auxiliary plant and generators) hereby permitted, when operating at its noisiest, shall not at any time exceed a value of 15 dB below the minimum external background noise, at a point 1 metre outside any window of any residential and other noise sensitive property, unless and until a fixed maximum noise level is approved in writing by the City Council. The background level should be expressed in terms of the lowest LA90, 15 mins during the proposed hours of operation. The plant-specific noise level should be expressed as LAeqTm, and shall be representative of the plant operating at its maximum. , (3) Following installation of the plant and equipment, you may apply in writing to the City Council for a fixed maximum noise level to be approved. This is to be done by submitting a further noise report confirming previous details and subsequent measurement data of the installed plant, including a proposed fixed noise level for written approval by the City Council. Your submission of a noise report must include:

(a) A schedule of all plant and equipment that formed part of this application; (b) Locations of the plant and machinery and associated: ducting; attenuation and damping equipment; (c) Manufacturer specifications of sound emissions in octave or third octave detail; (d) The location of most affected noise sensitive receptor location and the most affected window of it; (e) Distances between plant & equipment and receptor location/s and any mitigating features that may attenuate the sound level received at the most affected receptor location; (f) Measurements of existing LA90, 15 mins levels recorded one metre outside and in front of the window referred to in (d) above (or a suitable representative position), at times when background noise is at its lowest during hours when the plant and equipment will operate. This acoustic survey to be conducted in conformity to BS 7445 in respect of measurement methodology and procedures; (g) The lowest existing LA90, 15 mins measurement recorded under (f) above; (h) Measurement evidence and any calculations demonstrating that plant and equipment complies with the planning condition; (i) The proposed maximum noise level to be emitted by the plant and equipment. (C46AC)

Reason:

Because existing external ambient noise levels exceed WHO Guideline Levels, and as set out in Policies 7 and 33 of the City Plan 2019 - 2040 (April 2021) and the Environmental Supplementary Planning Document (February 2022), so that the noise environment of people in noise sensitive receptors is protected, including the intrusiveness of tonal and impulsive sounds, and by contributing to reducing excessive ambient noise levels. Part (3) is included so that applicants may ask subsequently for a fixed maximum noise level to be approved in case ambient noise levels reduce at any time after implementation of the planning permission. (R46AC)

- 8 No vibration shall be transmitted to adjoining or other premises and structures through the building structure and fabric of this development as to cause a vibration dose value of greater than 0.4m/s (1.75) 16 hour daytime nor 0.2m/s (1.75) 8 hour night-time as defined by BS 6472 (2008) in any part of a residential and other noise sensitive property. (C48AB)

Reason:

To ensure that the development is designed to prevent structural transmission of noise or vibration and to prevent adverse effects as a result of vibration on the noise environment in accordance with Policies 7 and 33 of the City Plan 2019 - 2040 (April 2021) and the Environmental Supplementary Planning Document (February 2022). (R48AB)

- 9 You must not put any machinery or associated equipment, ducts, tanks, satellite or radio aerials or telecommunication equipment on the roof, except those shown on the approved drawings, without our permission.

Reason:

Because without full assessment as to the principle and their siting and appearance, these may harm the appearance of this unlisted building of merit, the setting of nearby listed buildings and the character and appearance of the Bayswater Conservation Area and area generally, including views from the new Paddington Square Public Realm,

which are protected by Policies 38 and 40 of the City Plan 2019 - 2040 (April 2021).

- 10 The development hereby approved shall achieve a BREEAM rating of 'Excellent' or higher or an equivalent independent measure of energy performance and sustainability. Where the performance of the development is measured using BREEAM, it shall achieve not less than the total credits for each of the Energy, Materials and Waste categories in the BREEAM Pre-Assessment hereby approved. A post completion certificate (or equivalent certification) confirming that the development has been completed in accordance with the required BREEAM rating and has maintained or exceeded the approved total credit scores for each of the Energy, Materials and Waste categories, shall be submitted to us for our approval within three months of first occupation of the development. (C44BC)

Reason:

To make sure that the development provides the environmental sustainability features included in your application as set out in Policies 36 and 38 of the City Plan 2019 - 2040 (April 2021). (R44AD)

- 11 You must apply to us for approval of a detailed written and photographic specification of the materials you propose to use, including marked up versions of the approved elevations and roof plans showing where / how each of those materials are proposed to be used. For brickwork and other masonry, including restored retained masonry, you must include a minimum 1m² trial panel of each masonry type, prepared on-site for our inspection, which must also be recorded as part of your submitted specification. You must not start work on the relevant part of the development until we have approved in writing what you have sent us. You must then carry out the work using the approved materials.

Reason:

To make sure that the appearance of the building is suitable and that it contributes to the character and appearance of this part of the Bayswater Conservation Area. This is as set out in Policies 38, 39 and 40 of the City Plan 2019 - 2040 (April 2021). (R26BF)

- 12 You must apply to us for approval of further information (as set out respectively below), about the following parts of the development:
- (a). New windows and doors, including shopfronts, and including their surrounds and associated railings / pot-guards (detailed elevations and sections at 1:10, with key details at 1:5).
 - (b). New external stucco details (detailed elevations and sections at 1:10).
 - (c). New roof and dormers (detailed elevations and sections at 1:20).
 - (d). Layout and appearance of new plant (detailed elevations, plans and sections at 1:20, plus materials and finishes specifications).

You must not start any work on these parts of the development until we have approved what you have sent us. You must then carry out the work according to these details.

Reason:

To make sure that the appearance of the building is suitable and that it contributes to

the character and appearance of this part of the Bayswater Conservation Area. This is as set out in Policies 38, 39 and 40 of the City Plan 2019 - 2040 (April 2021). (R26BF)

- 13 **Pre Commencement Condition.** Notwithstanding the details submitted in relation to greening proposals and irrigation. You must apply to us for approval of the following details:-
- A. Green Wall Details including:-
 - Planting arrangement - intimate mix
 - Planting density of climbers (should be increased)
 - Initial planting height of climbers (should be increased).
 - Species -(should be 3 species of climbers, 2 of which should be evergreen)
 - Grid arrangement - (diagonal grid of wires not visually appropriate),
 - B. Green Infrastructure Management Plan including:-
 - Frequency and timing of maintenance
 - Responsibility of maintenance
 - Access arrangements
 - Details of sustainable irrigation.

Reason:

To improve the appearance of the development, to make sure that it contributes to the character and appearance of the area, and to improve its contribution to biodiversity and the local environment. This is as set out in Policies 34 and 38 of the City Plan 2019 - 2040 (April 2021). (R30BD)

- 14 You must not use any part of the development until we have approved in writing appropriate arrangements to secure the following:-
- Mitigation for the demand for cycle parking.
- In the case of each of the above benefit, you must include in the arrangements details of when you will provide the benefit, and how you will guarantee this timing. You must only carry out the development according to the approved arrangements. (C19BB)

Reason:

To make sure that the development provides mitigation for the demand for cycle parking in accordance with Policy 25 of the City Plan 2019 - 2040 (April 2021). (R22FB)

Informative(s):

- 1 In dealing with this application the City Council has implemented the requirement in the National Planning Policy Framework to work with the applicant in a positive and proactive way. We have made available detailed advice in the form of our statutory policies in the City Plan 2019 - 2040 (April 2021), neighbourhood plan (where relevant), supplementary planning documents, the London Plan (March 2021), planning briefs and other informal written guidance, as well as offering a full pre application advice service, in order to ensure that applicant has been given every opportunity to submit an application which is likely to be considered favourably. In

addition, where appropriate, further guidance was offered to the applicant at the validation stage.

- 2 NETWORK RAIL, , SAFETY, any works on this land will need to be undertaken following engagement with Asset Protection to determine the interface with Network Rail assets, buried or otherwise and by entering into a Basis Asset Protection Agreement, if required, with a minimum of 3months notice before works start. Initially the outside party should contact , assetprotectionwestern@networkrail.co.uk., If the project does require information/examination reports on our structures (including London St.) they can be provided at , request., , PLANT, SCAFFOLDING AND CRANES, Any scaffold which is to be constructed adjacent to the railway must be erected in such a manner that, at no time will any , poles or cranes over-sail or fall onto the railway. All plant and scaffolding must be positioned, that in the event of failure, it , will not fall on to Network Rail land.
- 3 When carrying out building work you must take appropriate steps to reduce noise and prevent nuisance from dust. The planning permission for the development may include specific conditions relating to noise control, hours of work and consideration to minimising noise and vibration from construction should be given at planning application stage. You may wish to contact to our Environmental Sciences Team (email: environmentalsciences2@westminster.gov.uk) to make sure that you meet all the requirements before you draw up contracts for demolition and building work. , , When a contractor is appointed, they may also wish to make contact with the Environmental Sciences Team before starting work. The contractor can formally apply for consent for prior approval under Section 61, Control of Pollution Act 1974. Prior permission must be sought for all noisy demolition and construction activities outside of core hours on all sites. If no prior permission is sought where it is required the authority may serve a notice on the site/works setting conditions of permitted work (Section 60, Control of Pollution Act 1974)., , British Standard 5228:2014 'Code of practice for noise and vibration control on construction and open sites' has been recognised by Statutory Order as the accepted guidance for noise control during construction work., , An action in statutory nuisance can be brought by a member of the public even if the works are being carried out in accordance with a prior approval or a notice.
- 4 Under the Construction (Design and Management) Regulations 2015, clients, the CDM Coordinator, designers and contractors must plan, co-ordinate and manage health and safety throughout all stages of a building project. By law, designers must consider the following: , , * Hazards to safety must be avoided if it is reasonably practicable to do so or the risks of the hazard arising be reduced to a safe level if avoidance is not possible;, , * This not only relates to the building project itself but also to all aspects of the use of the completed building: any fixed workplaces (for example offices, shops, factories, schools etc) which are to be constructed must comply, in respect of their design and the materials used, with any requirements of the Workplace (Health, Safety and Welfare) Regulations 1992. At the design stage particular attention must be given to incorporate safe schemes for the methods of cleaning windows and for preventing falls during maintenance such as for any high level plant., , Preparing a health and safety file is an important part of the regulations. This is a record of information for the client or person using the building, and tells them about the risks that have to be managed during future maintenance, repairs or renovation. For more information, visit the Health and Safety Executive website at www.hse.gov.uk/risk/index.htm. , , It is now possible for local authorities to

prosecute any of the relevant parties with respect to noncompliance with the CDM Regulations after the completion of a building project, particularly if such noncompliance has resulted in a death or major injury.

- 5 Regulation 12 of the Workplace (Health, Safety and Welfare) Regulations 1992 requires that every floor in a workplace shall be constructed in such a way which makes it suitable for use. Floors which are likely to get wet or to be subject to spillages must be of a type which does not become unduly slippery. A slip-resistant coating must be applied where necessary. You must also ensure that floors have effective means of drainage where necessary. The flooring must be fitted correctly and properly maintained., Regulation 6 (4)(a) Schedule 1(d) states that a place of work should possess suitable and sufficient means for preventing a fall. You must therefore ensure the following: * Stairs are constructed to help prevent a fall on the staircase; you must consider stair rises and treads as well as any landings; * Stairs have appropriately highlighted grip nosing so as to differentiate each step and provide sufficient grip to help prevent a fall on the staircase; * Any changes of level, such as a step between floors, which are not obvious, are marked to make them conspicuous. The markings must be fitted correctly and properly maintained; * Any staircases are constructed so that they are wide enough in order to provide sufficient handrails, and that these are installed correctly and properly maintained. Additional handrails should be provided down the centre of particularly wide staircases where necessary; * Stairs are suitably and sufficiently lit, and lit in such a way that shadows are not cast over the main part of the treads.
- 6 Working at height remains one of the biggest causes of fatalities and major injuries. You should carefully consider the following., * Window cleaning - where possible, install windows that can be cleaned safely from within the building., * Internal atria - design these spaces so that glazing can be safely cleaned and maintained., * Lighting - ensure luminaires can be safely accessed for replacement., * Roof plant - provide safe access including walkways and roof edge protection where necessary (but these may need further planning permission)., More guidance can be found on the Health and Safety Executive website at www.hse.gov.uk/toolbox/height.htm, , Note: Window cleaning cradles and tracking should blend in as much as possible with the appearance of the building when not in use. If you decide to use equipment not shown in your drawings which will affect the appearance of the building, you will need to apply separately for planning permission. (I80CB)
- 7 You are advised to permanently mark the plant/ machinery hereby approved with the details of this permission (including date decision and planning reference number). This will assist in future monitoring of the equipment by the City Council if and when complaints are received.

Please note: the full text for informatives can be found in the Council's Conditions, Reasons & Policies handbook, copies of which can be found in the Committee Room whilst the meeting is in progress, and on the Council's website.